

**Wivenhoe Goods Shed
De-listing Assessment
Prepared for Network Rail
July 2016**



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1.0 Introduction

This report has been commissioned by Network Rail to support an application to Historic England to remove Wivenhoe Goods Shed from the Statutory List. The shed is adjacent to Wivenhoe Station on the Colchester to Clacton railway line in Essex. The goods shed was likely constructed in the early C20.

The building was closed to rail traffic in 1965. Attempts to find a new use for the goods shed have been unsuccessful. Since closure, some of the building's significant features were lost, as shown in a 2014 photographic inspection. The goods shed suffered arson damage in September 2015. Following this damage, Network Rail undertook an inspection of the building and had to demolish the north end wall on safety grounds.

All that remains of the main goods shed is three of its exterior walls and its small office outbuilding. In cases of fire damage Historic England guidance states that 'extra care' is needed to ascertain whether a building retains its special interest. The guidance further states that applicants must show 'as much supporting evidence as possible' to demonstrate why a building has lost its special architectural or historic interest.

This report supplies this supporting evidence. It examines the history of Wivenhoe Goods Shed and details its current condition, together with photographs recording the condition of the goods shed on a site visit undertaken on 6 May 2016. It also demonstrates that Network Rail has taken the 'extra care' to ascertain whether Wivenhoe Goods Shed retains its special interest.



Figure 1: Site location

2.0 Understanding Wivenhoe Goods Shed

2.1 The history of the route

1863	Hythe to Wivenhoe route completed by Tendring Hundred Railway Wivenhoe to Brightlingsea route completed by the Wivenhoe and Brightlingsea Railway
1866	Wivenhoe to Weeley, East Gate and Hythe to Colchester St Botolph's completed by Tendring Hundred Railway
1878	Thorpe-le-Soken to Clacton with spur at Walton completed by the Clacton Railway
1883	route becomes part of the Great Eastern Railway (GER)
1963	Wivenhoe to Brightlingsea route closed
1965	Wivenhoe Goods shed closed

The railway line from Hythe to Wivenhoe was constructed by the Tendring Hundred Railway Company. Completed and opened in 1863, the railway was worked by the Great Eastern Railway (GER). At Wivenhoe, the Wivenhoe and Brightlingsea Railway built an end-on junction.

The Tendring Hundred Railway was 'interested in expansion' (Phillips, p.6) especially towards Walton-on-the-Naze. In 1866 a single track line from East Gate and Hythe Junctions to a new Colchester station at St Botolph's (later renamed Colchester Town Station) was completed. The extension to Colchester led to a 'large and steady increase' in through and local traffic which soon showed 'the great utility of the lines and the convenience of the new terminus.' As *The Railway News* reported, during the 13 weeks ending 2 September, 42,649 passengers had used the branch, compared to just 13,640 in the same period the previous year.

The railway was also extended from Wivenhoe to Weeley in January 1866, and the Tendring Hundred completed the branch line to Brightlingsea (also operated by the GER). The following May, the Tendring Hundred Railway reached Walton. The new lines were single tracked.

In 1876, the Wivenhoe and Brightlingsea fell out with the GER, and began to run its branch line itself. The Tendring Hundred Railway, which had no argument with the GER, refused to let the Wivenhoe and Brightlingsea use its station at Wivenhoe. In response a new station was constructed. This second Wivenhoe Station had a short life and was closed after the dispute was resolved. The GER resumed running services to Brightlingsea in 1879.

In July 1878 the Clacton Railway started construction work on a railway line from Thorpe-le-Soken to Clacton, with a spur to the Tendring Hundred at Walton. This opened in July 1882.

The GER 'did not like merely working lines – it also liked to own them' (Phillips, p. 10). In June 1883, the Tendring Hundred Railway and the Clacton Railway were amalgamated into the GER. The Wivenhoe and Brightlingsea Railway was amalgamated into the GER in June 1893.

The line was doubled between Colchester and Wivenhoe by 1886 and from Great Bentley to Thorpe in 1890. After the rationalisation of the smaller railway companies after the First World War, the GER became part of the London & North Eastern Railway (LNER) in 1923. The LNER doubled the line from Thorpe-le-Soken to Clacton in 1936.

After nationalisation, the route became part of British Railways (Eastern Region). The route was completely electrified by March 1959, and regular steam hauled services ended in 1961.

The Beeching Report brought closures to the line. The branch line to Brightlingsea was closed in 1963 and the report recommended that 'all goods yards on the Clacton and Walton lines' should be closed 'to general traffic' (Phillips p. 45).

2.2 The history of Wivenhoe Goods Shed

The list description dates Wivenhoe Goods Shed to c. 1866. Its dating by Historic England (then English Heritage) links the shed to the expansion of the railway to Colchester and Weeley, as well as the completion of the branch line to Brightlingsea. It was a period when the line was experiencing an increase in rail traffic. The goods shed forms part of a group of railway buildings – which includes the former stables (dated by Historic England to c. 1860 and listed Grade II) and the railway station (rebuilt by the GER in 1886).

However, there have been other opinions on the dating of Wivenhoe Goods Shed. Gordon Biddle's *Britain's Historic Railway Buildings* dates the goods shed to 1903, while the Wivenhoe Encyclopaedia (an online resource) dates the shed to 1902.

The dating of railway structures such as goods sheds can be difficult. Stylistically, features at Wivenhoe Goods Shed which might indicate an earlier construction do not necessarily provide convincing evidence. At Wivenhoe, the timber queen-post roof with iron bracing (figure 9) potentially indicates a construction date of c.1860s. This same roof construction is found at Bedford Goods Shed (figure 10), a substantial Midland Railway construction built c.1890.



Figure 2: Wivenhoe Station rebuilt by the GER in 1886



Figure 3: Former stables, dated by Historic England to c. 1860 (listed Grade II)

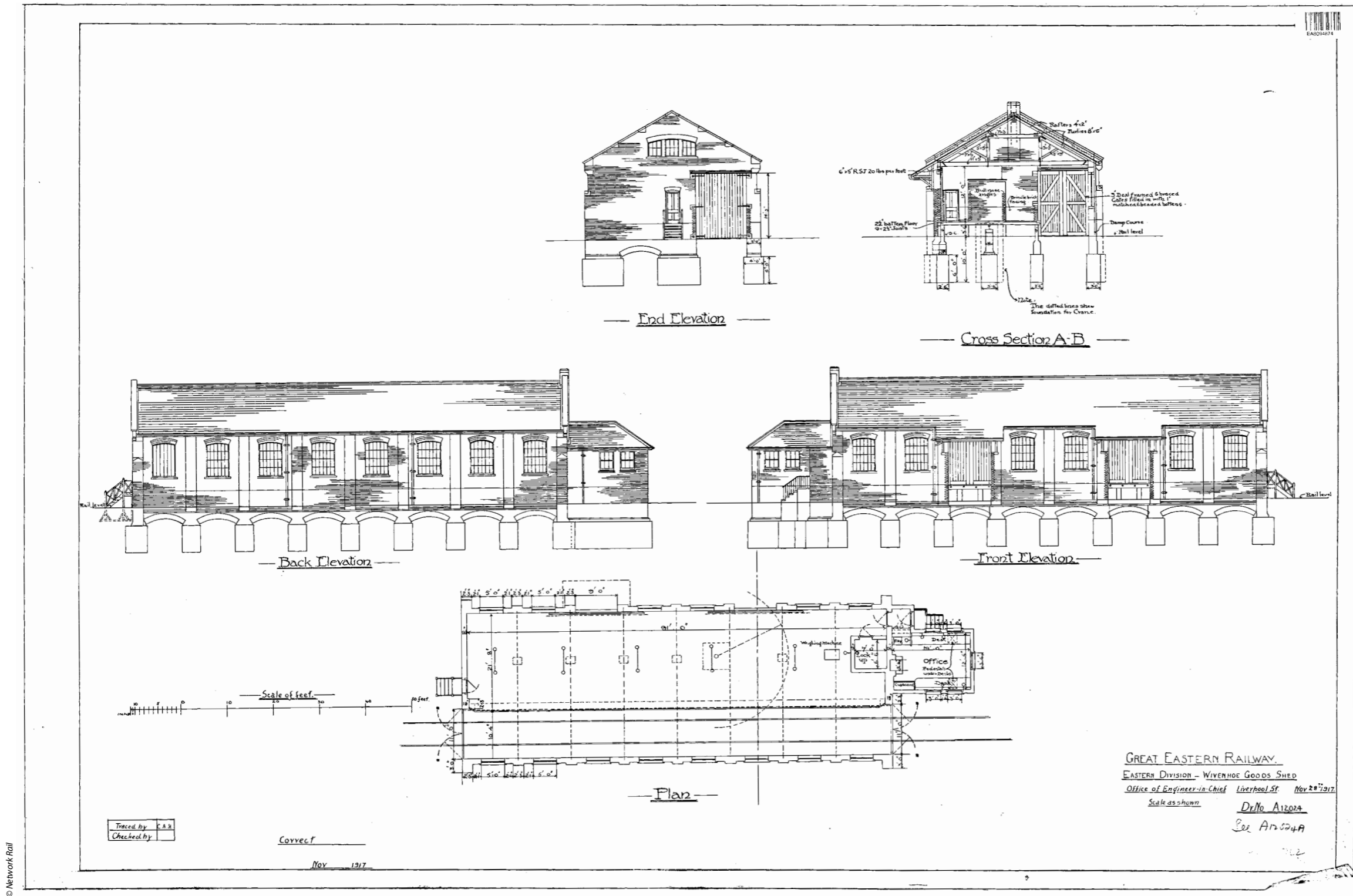


Figure 4: Plan of Wivenhoe Goods Shed (traced in 1917)

However, it is the mapping evidence which provides convincing evidence that the goods shed does not date to the 1860s. The earliest map to show the goods shed is dated 1874-75. A map of 1897 (figure 5) shows the goods shed in detail. The map shows that the goods shed had one large door on its east elevation, an extension at the north-east and a small office building to the south. This had changed by the time the plan of the goods shed and station was drawn up (figure 6). There is no sign of the extension at the north-east of the shed and the configuration of the small office building to the south has changed. This plan is dated to c.1900. The detail on the plan is confirmed by the next map of the area, dated 1923 (figure 7).

The design of the goods shed as built after c.1900 is clearly seen in the only surviving GER plan which has come to light. Traced from an existing plan in November 1917 (figure 4), it shows no sign of any demolished extension on the north-east and shows the two lean-to canopies with doors to the front elevation.

Also, an analysis of the 1897 and 1923 maps indicates that the rebuilt shed was located further eastward. Figure 8 shows an overlay of the two maps. In the map of 1897, one track is shown to the west of the goods shed but in the 1923 map (and also in the c. 1900 plan) a new track layout is shown with two tracks located to the west of the goods shed.

The evidence indicates that Wivenhoe Goods Shed was rebuilt at some point in the early C20 and at least before 1917. This dates the building to the fourth phase of railway construction, the completion of the network – a period from the end of the third phase in the 1870s up to 1914. Goods sheds are classified as the third category of railway structures ‘that were adapted for railway use.’ The Listing Selection Guide: Transport Buildings (April 2011) sets out the criteria for assessing railway structures after 1860:

Increasingly rigorous selection is required for buildings after about 1860: this reflects both the quantity of what remains, and the standardisation of design which was applied to buildings and structures erected along different railway lines.

The guidance further states that other factors should be taken into account ‘when assessing buildings of the latter half of the nineteenth century’ – including different company house styles and rarity. In the case of Wivenhoe Goods Shed, the evidence indicates the building does not date to the later C19 but probably from the early C20. This means that a more rigorous selection should be applied to Wivenhoe Goods Shed, which is a standardised early C20 example of railway design.



Figure 5: 1897 map showing Wivenhoe Goods Shed

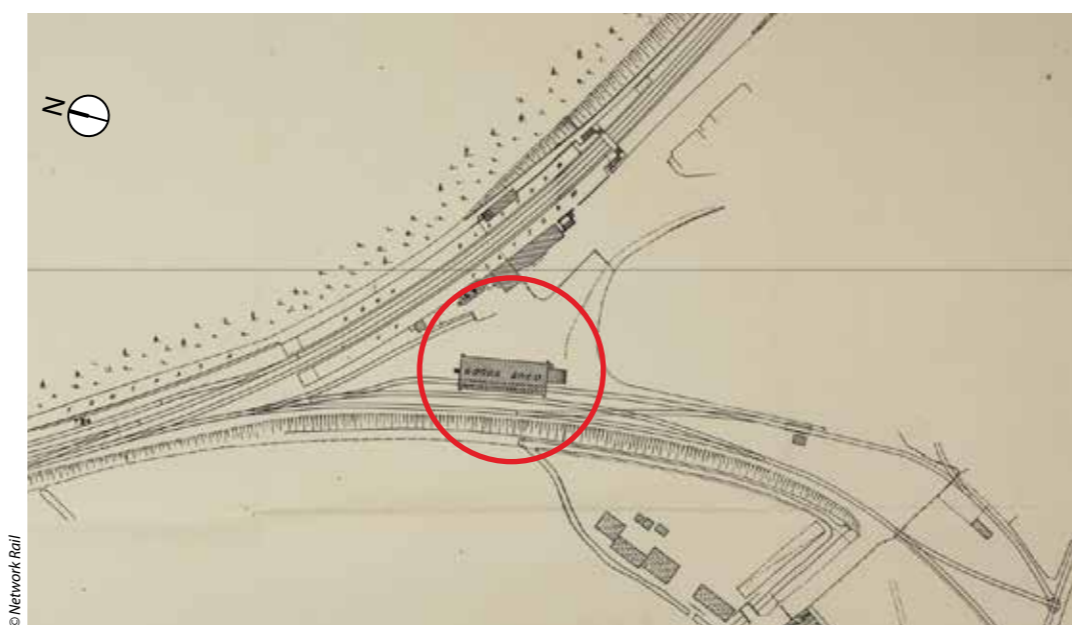


Figure 6: Wivenhoe: Plan of Station and Goods Yard (dated to c.1900) by courtesy of Essex Record Office D/Z 346/3250/2

2.3 Wivenhoe Goods Shed since 1965

In the 1960s, following the Beeching Report, the goods sheds along the Colchester to Clacton line were taken out of service. The goods shed at Wivenhoe closed on 4 October 1965.

Attempts to reuse the building have proved unsuccessful. In 2004 a community group wanted to convert the vacant goods shed into an auditorium and 'cultural hub' but the projected cost of £750,000 was prohibitively expensive.

A press report noted that the East Anglian Railway Museum was interested in acquiring the goods shed at about the same time 'when the roof covering had come off but the timbers were reasonably sound, with a view to dismantling and relocating to the museum.' However, this also came to nothing.

Biddle noted that the goods shed 'was badly damaged by fire' in 2000. Certainly before the arson attack in 2015 the goods shed had lost two of its principal features – the slate roof covering and the large end doors. These end doors to both the north and south elevations were replaced by modern steel replacements, which survived the arson attack. A Network Rail inspection of the building in March 2014 shows much of the building's features intact, although the condition of the queen-post roof had deteriorated. The inspection also showed subsidence affecting the north-west corner of the goods shed, with cracking evident in the wall structure.

The arson attack in September 2015 caused substantial damage to Wivenhoe Goods Shed: the roof was burnt off, the lean-to timber doors destroyed, the lean-to canopies largely lost and the interior reduced to an empty shell. Network Rail inspected the building and concluded that the north end wall had to be demolished on safety grounds. This was carried out in October 2015.

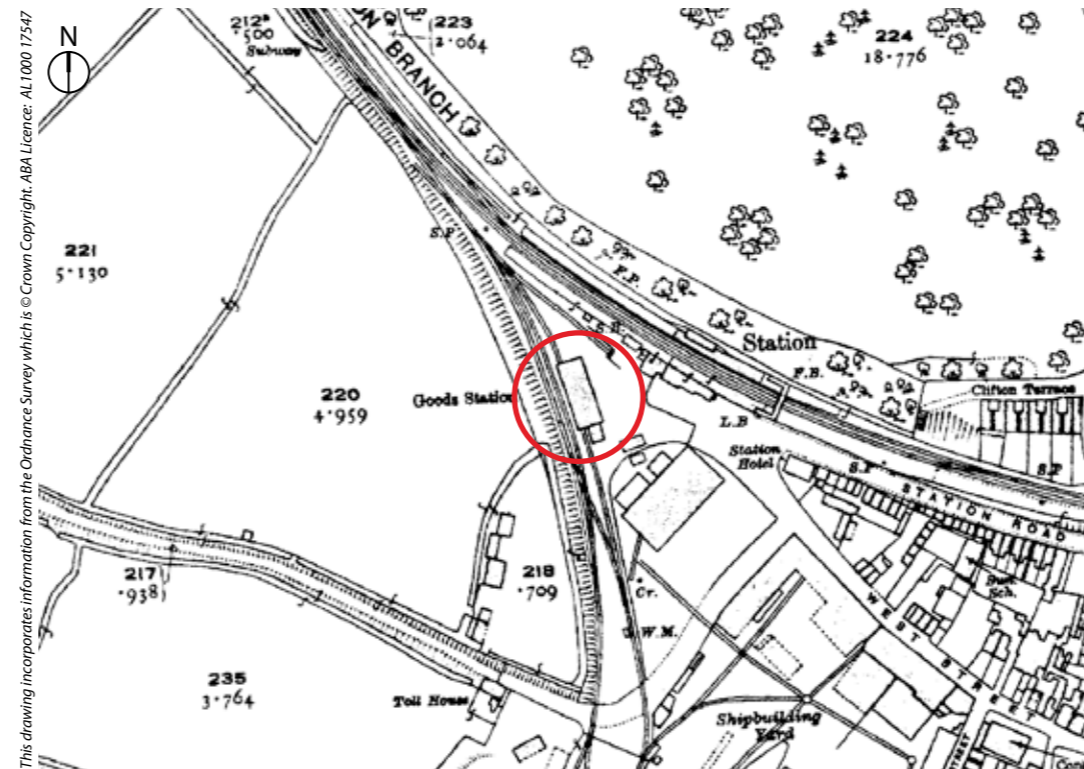


Figure 7: 1923 map showing Wivenhoe Goods Shed

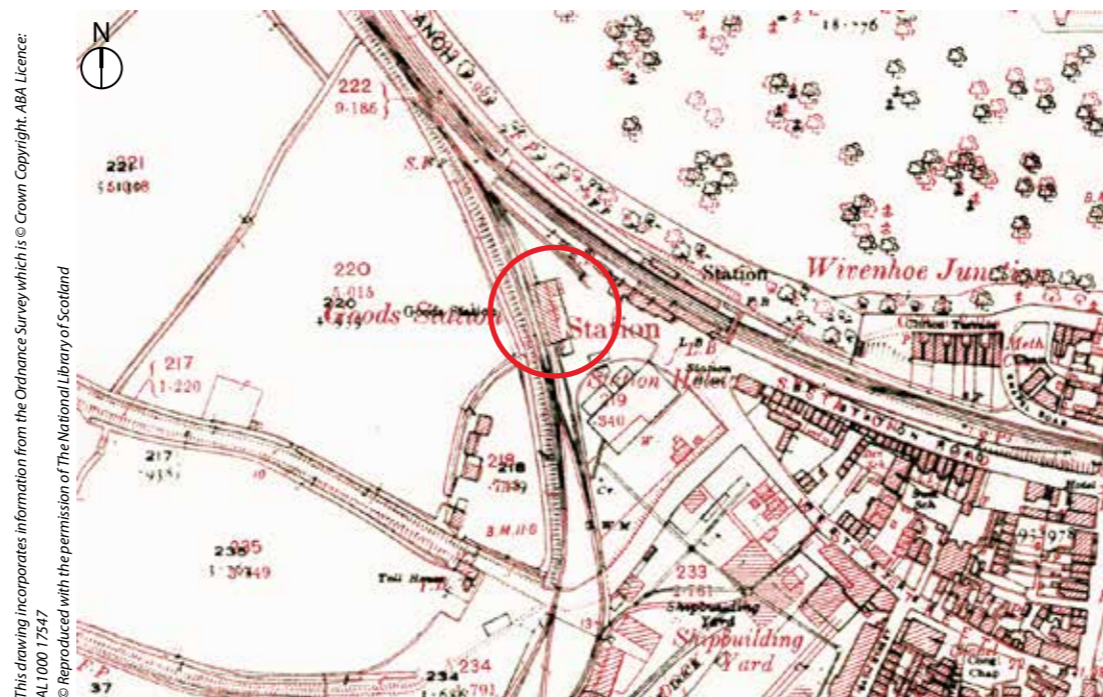


Figure 8: Overlay of 1897 (red) and 1923 (black) maps

3.0 De-listing

3.1 Designation of Wivenhoe Goods Shed

Wivenhoe Goods Shed was listed Grade II as 'Goods Shed to South West South of Wivenhoe Station' on 15 August 1988. The list description states:

English bond brick; gabled Welsh slate roof, with moulded stone kneelers to stone coping. One storey; 8 bays. Raised pilasters separate each bay. Segmental brick arches over small-pane cast-iron casements. Lean-to canopies, supported by timber brackets, over 2 mid C19 sliding doors with counterweights. Similar fenestration to rear elevation. Segmental-arched tripartite windows in gable ends. Interior: 8 bay queen-post roof, of softwood with iron bracing.

3.2 De-listing guidance

Historic England's guidance for de-listing is set out in 'Guidance for De-listing a Building' published on 27 November 2015. There are only two criteria which can be considered for de-listing:

An application for de-listing may be made because new evidence is available about the lack of special architectural or historic interest of the building, or a material change of circumstances, for example fire damage.

The guidance notes the level of detail required for applications to de-list:

Applicants will need to provide as much supporting evidence as possible to show why it is believed a building no longer has special architectural or historic interest. This should include written information, photographs and maps where appropriate.

In considering any request for de-listing, Historic England 'can only consider comments on the special architectural or historic interest of a building.' The guidance also specifically addresses fire-damaged buildings:

Extra care is needed with fire-damaged buildings. Experience shows that special interest may remain even when considerable damage has been done to the fabric, both by fire and by use of the water to put the fire out. A de-listing application will not normally be processed until the cause of the fire have been established and enforcement action leading to some form of restoration or repair ruled out.

The de-listing guidance is clear that 'extra care' must be taken in cases of fire damage, and a site visit was undertaken to photograph the building in detail and ascertain whether Wivenhoe Goods Shed retains special interest.

3.3 New evidence: the dating of Wivenhoe Goods Shed

The list description makes no mention of any subsequent alterations to the goods shed, merely dating it to c. 1866. There is little surviving evidence to date the goods shed with certainty, and as stated at section 2.2 dating ancillary railway buildings can be a difficult exercise. For example, features which may indicate an earlier construction date can be found in later railway buildings.

However, the mapping evidence does indicate that Wivenhoe Goods Shed was rebuilt after 1897, likely in the early C20. The plan dated to c.1900 and the historic map of 1923 shows a different design and location to the shed shown in the earlier maps. There is no sign of the extension at the north-east of the shed, the configuration of the small office building to the south has changed and there are two lean-to canopies on the front (east) elevation. The traced 1917 plan of the goods shed also shows the two lean-to canopies with timber doors, while there is no sign of any former extension at the north east of the shed. The new office building is also shown in the traced plan.

There is convincing 'new evidence' that Wivenhoe Goods Shed was incorrectly 'identified as having special interest.' It is not a c.1860s shed built by the Tendring Hundred Railway but a probable early C20 shed constructed by the Great Eastern Railway. It is not an unaltered later C19 railway building.



Figure 9: Wivenhoe Goods Shed roof structure in 2014



Figure 10: Bedford Goods Shed built c.1890 roof structure (photographed 2014)

3.4 Material change of circumstances: the current condition of Wivenhoe Goods Shed

The Historic England advice cautions that 'It should not be assumed that the List description encompasses all aspects of the special interest for which the building was listed.' The older list descriptions particularly were used as 'tools for identifying a building, rather than an explanation of its special interest.' Nevertheless, the list description for Wivenhoe Goods Shed provides a brief but comprehensive list of the building's special features. This section analyses elements of the goods shed and illustrates its current condition.

Exterior:

The only part of the goods shed to survive relatively intact is the small office building and the south elevation.

Much, if not all, of the 'Welsh slate roof' had been lost by 2014. The arson attack of September 2015 completely destroyed whatever remained.

The north end wall was relatively intact when inspected in 2014. The 'tri-partite window' had been blocked up. Steel doors had replaced the original timber doors. An inspection by NR after the arson attack concluded that the north end wall required demolition. This work was carried out in October 2015.

Elsewhere the 'segmental brick arches' survive intact, and the 'small-pane cast iron casements' on the east elevation have survived but with areas of warping. The 'small-cast iron casements' on the west elevation have also been warped by the fire damage. These would have to be removed and replaced if any rebuilding work was contemplated.

The 'lean-to canopies' with timber brackets, together with 'heavy timber doors', together with the end doors, are a key component of the shed's significance. As noted earlier, the doors to the south elevation had been removed before 2015. The lean-to canopies on the east elevation at Wivenhoe Goods Shed have been destroyed by the fire, and while the timber brackets survive these are badly charred and would need to be replaced. The sliding doors have also been completely destroyed.



Figure 11: North end wall of Wivenhoe Goods Shed photographed in 2014



Figure 12: Exterior viewed from the north showing demolished elevation and loss of roof (2016)



Figure 13: Detail of south elevation seen from the station car park (2016)



Figure 14: Former roof line seen from the station car park (2016)



Figure 15: Detail of east elevation showing detail of damaged cast-iron casements (2016)



Figure 16: Detail of east elevation showing detail of damaged cast-iron casements (2016)



Figure 17: Detail of east elevation showing damaged cast-iron casements (2016)



Figure 18: Detail of east elevation showing damage to second lean-to canopy and loss of roof (2016)



Figure 19: Detail of east elevation showing damage to second lean-to canopy, damage to cast-iron casements and loss of roof (2016)



Figure 20: Detail of east elevation showing sixth bay window and loss of roof (2016)



Figure 21: Detail of west elevation showing damage to cast-iron casements on the seventh bay window and the less damaged eighth bay window (2016)



Figure 22: Detail of west elevation showing damaged cast-iron casements and loss of roof (2016)



Figure 23: Close-up detail of west elevation showing damaged cast-iron casements and loss of roof (2016)



Figure 24: West elevation under scaffolding (2016)



Figure 25: Detail of subsidence on west elevation (2016)



Figure 26: Detail of north-west corner showing subsidence (2016)



Figure 27: Detail of north-west corner showing subsidence and damaged cast-iron casements on first bay (2016)



Figure 28: Detail of damaged first lean-to canopy on east elevation (2016)



Figure 29: Close-up detail of former roof structure to first lean-to canopy on east elevation (2016)



Figure 30: Detail of damaged second lean-to canopy on east elevation (2016)



Figure 31: Close-up detail of former roof structure to second lean-to canopy on east elevation (2016)



Figure 32: Steel replacement doors on south elevation (2016)



Figure 33: Steel replacement doors on south elevation seen from interior (2016)



Figure 34: Small office building east elevation (2016)



Figure 35: Small office building south elevation (2016)



Figure 36: Small office building west elevation (2016)



Figure 37: Small office building details of roof on east elevation (2016)



Figure 38: Detail of south east elevation (2016)



Figure 39: Detail of south west elevation (2016)

Interior:

The main body of the goods shed is now largely an empty shell, with brick foundations visible adjacent to the south end wall. Fragments of charred timber and badly damaged iron work are also visible around the site.

The 2014 Network Rail inspection report showed that the handsome queen-post roof had deteriorated, but remained largely intact. This is the only interior feature detailed in the 1988 list description. The arson attack completely destroyed the roof structure as evident in the 2016 site visit photographs.

The 2014 NR inspection photographs show that much of the wooden platform had survived. Not mentioned in the list description, the platform would have been the working centre of the shed, with goods packed ready for onward transportation by railway or ready to be transferred to horse drawn and later mechanised road transport.

However, the platform has been irreparably lost as shown in the following photographs. The interior of the small office building potentially survives with original features intact. However, it was not possible to inspect this building during the site visit as the only access door was locked. The sash window frames could be viewed from the main body of the shed.



Figure 40: Goods shed platform photographed in 2014



Figure 41: Detail of east and south elevations showing loss of interior features (2016)



Figure 42: Detail of south elevation showing loss of roof structure (2016)



Figure 43: General shot of shed interior showing loss of fixtures and fittings (2016)



Figure 44: Close-up of east interior elevation showing lack of internal features (2016)



Figure 45: East interior elevation showing lack of internal features (2016)



Figure 46: Close-up of east interior elevation showing lack of internal features (2016)



Figure 47: West interior elevation showing damaged cast-iron casements and loss of roof (2016)



Figure 48: Close-up of damaged west interior elevation (2016)



Figure 49: Detail of damaged interior platform structure (2016)



Figure 50: Detail of damaged interior platform structure and steel doors (2016)



Figure 51: Detail of small office interior showing sash windows as seen from shed (2016)

4.0 Conclusion

Wivenhoe Goods Shed is part of a grouping of railway structures including the former stables and the rebuilt GER station at Wivenhoe in Essex. The goods shed closed to traffic in 1965. Since the goods shed was taken out of operational use, attempts to find a new use for the building have been unsuccessful.

There is strong evidence that the building is much later than described in the list entry. It seems to date in its current form to after 1897, not 1866, putting it in the fourth phase of railway construction, the completion of the network. This is 'new evidence [...] about the lack of special architectural or historic interest' as stated in Historic England's 'Guidance for De-listing a Building.'

There is also a 'material change of circumstances' to consider. The building fabric had deteriorated by the time of the photographic inspection in March 2014, the two end doors had been replaced, much of the slate roof had gone and the roof structure has suffered areas of damage. However, much of what gave Wivenhoe Goods Shed its special interest had survived.

The arson attack in September 2015 caused substantial damage to the goods shed, and destroyed those features which gave the building its special interest.

The condition of the goods shed exterior can be summarised as:

- Complete loss of the north end wall, loss of the south end doors, complete loss of the slate roof, severe damage to the lean-to canopies and loss of the timber doors, severe damage to the cast-iron casements roof and subsidence to the north-west corner.

- The small office building to the south elevation is the only element of the exterior/interior which has survived reasonably intact, but this survival is not enough to state that the building retains its special interest.
- The interior is now largely empty. The handsome queen-post roof structure was completely destroyed, as was the wooden platform which was the working centre of the goods shed until 1965, and all other fixtures, fittings and features.

The detailed site visit and photographs demonstrate that 'extra care' has been taken to ascertain the current condition of Wivenhoe Goods Shed. As the guidance states, even after fire damage, a listed building can retain special interest. However, in the case of Wivenhoe Goods Shed, it retains only three of its elevations, with evidence of subsidence to the north-west corner. Internally, it is little more than an empty shell.

There are two reasons why Wivenhoe Goods Shed should be de-listed: new evidence dating the goods shed to post-1897 and a material change of circumstances since September 2015. As such Wivenhoe Goods Shed has lost its special interest and should be removed from the List.

5.0 Sources

5.1 Primary sources

The Railway News, 29 September 1866

Network Rail Archives

Great Eastern Railway Eastern Division Wivenhoe Goods Shed (1917)

Essex Record Office

Wivenhoe: Plan of Station and Goods Yard (c.1900)

5.2 Secondary sources

Gordon Biddle (2003), Britain's Historic Railway Buildings

Charles Philips (1989), The Tendring hundred Railway. A History of the Colchester to Clacton and Walton lines

5.3 Web resources:

East Anglian Daily Times <http://www.eadt.co.uk> (accessed 20 May 2016)

Wivenhoe Encyclopaedia <http://www.wivencyclopedia.org/> (accessed 20 May 2016)

Appendix 1: List description

GOODS SHED TO SOUTH WEST SOUTH OF WIVENHOE STATION

List Entry Summary

This building is listed under the Planning (Listed Buildings and Conservation Areas) Act 1990 as amended for its special architectural or historic interest.

Name: GOODS SHED TO SOUTH WEST SOUTH OF WIVENHOE STATION

List entry Number: 1225568

Location

GOODS SHED TO SOUTH WEST SOUTH OF WIVENHOE STATION, STATION ROAD

The building may lie within the boundary of more than one authority.

County: Essex

District: Colchester

District Type: District Authority

Parish: Wivenhoe

National Park: Not applicable to this List entry.

Grade: II

Date first listed: 15-Aug-1988

Date of most recent amendment: Not applicable to this List entry.

Legacy System Information

The contents of this record have been generated from a legacy data system.

Legacy System: LBS

UID: 421988

Asset Groupings

This list entry does not comprise part of an Asset Grouping. Asset Groupings are not part of the official record but are added later for information.

List entry Description

Summary of Building

Legacy Record - This information may be included in the List Entry Details.

Reasons for Designation

Legacy Record - This information may be included in the List Entry Details.

History

Legacy Record - This information may be included in the List Entry Details.

Details

THE FOLLOWING ITEM SHALL BE ADDED

WIVENHOE STATION ROAD 1. 5214 TM 02 SW 13/54 Goods Shed to SWS of Wivenhoe Station II 2 Goods shed c 1866. English bond brick; gabled Welsh slate roof, with moulded stone kneelers to stone coping. One storey; 8 bays. Raised pilasters separate each bay. Segmental brick arches over small-pane cast-iron casements. Lean-to canopies, supported by timber brackets, over 2 mid C19 sliding doors with counterweights. Similar fenestration to rear elevation. Segmental-arched tripartite windows in gable ends. Interior: 8-bay queen-post roof, of softwood with iron bracing.

Listing NGR: TM0362121673

Selected Sources

Legacy Record - This information may be included in the List Entry Details

National Grid Reference: TM 03621 21673

Alan Baxter

Prepared by Reider Payne

Reviewed by Susannah Brooke

Draft issued June 2016

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